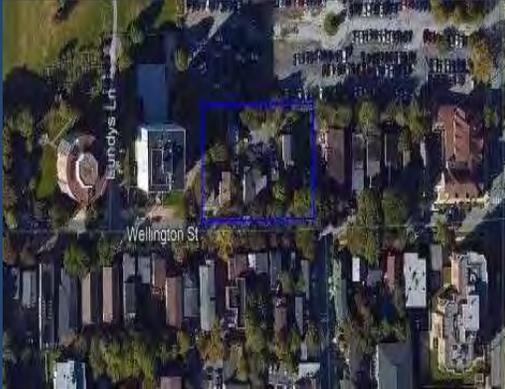


CONSTRUCTION MANAGEMENT PLAN

May 05, 2021



Wellington Apartments 1110-1132 Wellington Avenue, Halifax, NS

Project Number 15-101



PREPARED BY:

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PREPARED FOR:

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SECTION 1 – INTRODUCTION

SECTION 1.1 – PROJECT DESCRIPTION AND OBJECTIVES

The Wellington apartments Project at 1110 to 1132 Wellington Street will be a residential building abutting Wellington Street in the Halifax South Downtown district of Halifax Regional Municipality (HRM). The building is situated on a property with a total area of 1841m² and has 9 storeys with 101 residential units and two levels of underground parking (93 parking spaces total) with a 9th-floor amenity room, an outdoor terrace, and a gym.

Banc Investments Limited (BIL) is both the owner and General contractor of the project and, along with its architect, Lydon Lynch, and Eastern Traffic Services, have provided the necessary documents for this Construction Management Plan (CMP) which has been prepared and reviewed by DesignPoint Engineering & Surveying Ltd. to reduce potential negative impacts on the surrounding community. This CMP intends to outline all construction activities for the duration of the project to mitigate the effects to the surrounding neighborhood before they arise and to address any unforeseen issues.

Should any changes be required to this CMP, an email and hard- copy of the proposed amendments shall be sent to HRM for review a minimum of 10 days before the proposed implementation. Changes will only be implemented following approval from HRM.

SECTION 1.2 – PROJECT CONTACT INFORMATION

The project team for the proposed development consists of:

Developer/ Contractor:	BIL 1 Craigmore Drive, Suite 201, Halifax, NS (902)431-4901 reception@bancgroup.ca Alex Halef, <i>President</i> (902) 830-6406 (<i>cell</i>) ahalef@bedfordcommons.ca Sakir Ali, <i>Project Coordinator</i> (902)441-8193 projectcoordinator@bancgroup.ca
Architect:	Lydon Lynch Architects Limited 1668 Barrington Street, Unit 401

Halifax, Nova Scotia, B3J 2A2
(902)422-5454

Traffic Management: Eastern Traffic Services
John Smith, Traffic Control Manager (TCM)
johns@easterntraffic.ca

Rodent Control Services: Mike Payzant, Service Technician
Target Pest Control
1-902-817-9200
Targetpest.ca

SECTION 1.3 – GENERAL NOTES

This CMP has been prepared to comply with 2018-005-ADM from HRM and shall accompany the permit application, along with the applicable fee(s) as set out in bylaw F-200 and Administrative Order 15.

Any Significant departure from this CMP shall be submitted to HRM in writing at least 10 business days before implementation. In the event of unforeseen circumstance which shall require a change to the CMP for less than 24 hours, the departure will be put in effect immediately. HRM will then be notified as soon as possible. If the project changes ownership or contracting services, the Municipality shall be notified, and the new parties shall agree to comply with an approved CMP in writing.

When applying for the permit for this project, the following shall also be submitted to HRM:

- detailed building drawings, including engineering, landscaping, and architectural plans;
- applicable fees;
- detailed Construction Management Plan (CMP);

BIL will submit this CMP and encroachment applications in advance of development construction. Once Submitted, This CMP shall be evaluated by HRM as part of the building permit review process. Construction shall not begin until a permit has been issued, as required by the provisions of the HRM Streets By-Law (S-300).

The approved CMP shall be kept on-site at all times.

Some of the requirements in the Administrative Order will not apply to this project. In each case, an explanation is provided; deviations are subject to HRM approval.

The contractor or owner shall inspect the implementation of the CMP daily at minimum and keep a detailed inspection and maintenance log.

SECTION 2 – CONSTRUCTION SCHEDULE AND LOGISTICS

SECTION 2.1 – SCHEDULE

- Demolition (Jan 1 – Jan 31, 2021) 1 Month
- Excavation (Feb 1, 2021 – July 31, 2021) 7 Months
- Substructure (August 1, 2021 – June 30, 2022) 11 Months
- Superstructure (July 1, 2019 – Dec 30, 2020) 18 Months

From start to completion, the project will take approximately 3 years. See below for phasing timelines.

Project Phase	2021												2022												2023																
	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D	J	F	M	A	M	J	J	A	S	O	N	D					
Demolition	3m																																								
Excavation				EXCAVATION (9 Months)																																					
Sub-structure												SUB-STRUCTURE(13 Months)																													
Super-structure																								SUPER-STRUCTURE(19 Months)																	

SECTION 2.2 – WORK WITHIN THE PUBLIC RIGHT-OF-WAY

Each project phase will require sidewalk encroachments within the Halifax Regional Municipality (HRM) right-of-way (ROW) and will remain in place for the entire project. See section 2.2.1

Barrier installation will take place within the sidewalk areas, the installation of barriers is expected to take place on May 1st, 2021. See Appendix A for the barrier installation plan.

All Sidewalk Encroachments will be in place until the completion of the building’s façade. When the encroachments are no longer needed, and once the site is secure, the public ROW will be reinstated, cleaned, and turned back over to the Municipality. HRM will be given 5 business days notice before reinstatement.

During the Substructure phase, Wellington street will be temporarily closed to facilitate the installation of services. The excavation contractor will apply for a street closure permit and will follow all notification requirements as outlined in Section 8.3. This work is anticipated to take place in May 2022.

During the Superstructure phase, the crane erection and dismantle will take place using the South street parking lot entrance. There will not be any impact on Wellington street to close to facilitate the Crane disassembly.

During the Superstructure phase, Natural Gas connection will be done in Wellington street. Heritage Gas will apply for a street closure permit and will follow all notification requirements as outlined in Section 8.3. This work is anticipated to take place in March 2023. No Closure will be required, traffic control personnel will direct vehicles around the work.

All work to be carried out within the Public Right-of-Way will not proceed unless approved by HRM, and proper notification has been given. See Section 8.3.

Section 2.2.1 – Encroachments

Section 2.2.1.1 - Wellington Street

The Wellington Street Encroachment will not be used for site entry/exit for the duration of the project.

SECTION 2.3 – HOURS

Construction activities will adhere to the HRM noise bylaw for residential areas.

SECTION 3 – RELEVANT REGULATIONS & GUIDELINES

SECTION 3.1 – OCCUPATIONAL HEALTH & SAFETY REGULATIONS

This CMP shall be utilized in agreement with all applicable Provincial and Federal Occupational Health and Safety Regulations. At a minimum, construction activities must at all times, meet the standards of:

- National Building Code of Canada, as adopted and modified under the Building Code Act and the Nova Scotia Building Code Regulations made under that Act;
- Nova Scotia Occupational Health and Safety Act, and the Nova Scotia Occupational Safety General Regulations made under that Act;
- The Transportation Association of Canada (TAC)'s Manual of Uniform Traffic Control Devices for Canada (MUTCDC); and
- Nova Scotia Temporary Workplace Traffic Control Manual (NSTCM).

SECTION 3.2 – MUNICIPAL REGULATIONS & GUIDELINES

In addition to the Provincial and Federal standards referenced in Section 3.1, this CMP shall be utilized in agreement with and meet, at a minimum, the standards of all relevant municipal by-laws including, but not limited to, the following:

- S-300 Streets;
- E-200 Encroachments;
- B-201 Building;
- N-200 Noise;
- T-600 Trees;
- S-900 Controlled Access Streets;
- T-400 Truck Routes;
- W-101 Discharge into Public Sewers;
- B-600 Blasting; and
- HRM TCM Supplement.

SECTION 4 – VEHICLE AND PEDESTRIAN MANAGEMENT

SECTION 4.1 – VEHICULAR TRAFFIC CONTROL

Before any construction activity, all temporary workplace traffic control devices and signage will be in place as per the Nova Scotia Temporary Workplace Traffic Control Manual (latest edition). See appendices A, B and C.

Section 4.1.1 – Payment of Applicable Fees

Payment of all applicable fees will be made in accordance with HRM Administrative Order 15 (AO15). Requests for lane and street closures must be submitted to HRM a minimum of 10 days prior to their planned implementation for review and approval.

Section 4.1.2 – Vehicular Hazard Assessment

See Appendix D for vehicular and pedestrian hazard assessment.

Section 4.1.3 – Traffic Control Plan Preparation & Monitoring

Separate Traffic Control Plans (TCPs) have been prepared as required for project in accordance with the standards identified in the TAC Manual of Uniform Traffic Control Devices, the NSTIR Temporary Workplace Traffic Control Manual, and the HRM Traffic Control Manual Supplement.

TCPs have been prepared by Eastern Traffic certified temporary workplace signer (TWS), John Smith. The TCPs illustrate the proposed signage, which will be installed to warn vehicular traffic ahead of, and throughout the construction zone. Traffic control features and methods, as well as information related to the TCPs, are provided in Section 4.1.8.

Section 4.1.4 – Notifications of Traffic Closures

The contractor shall notify HRM and the public to proposed traffic closures as outlined in Section 8.3.

Section 4.1.5 – Traffic Control Element Inspection & Maintenance

All TCPs will be implemented by Eastern Traffic and its team of certified Traffic Control Persons (as recognized by the Nova Scotia Department of Transportation and Infrastructure Renewal). Construction warning signage will be displayed throughout the approaches to and adjacent to the project site following the Nova Scotia Temporary Workplace Traffic Control Manual. BIL will inspect traffic control elements at the start and end of the workday, or more frequently as required. It will maintain traffic control elements as required to ensure that the TCPs are effectively and correctly implemented. CMP element inspection sheets to be completed by BIL shown in See Appendix E.

Section 4.1.6 – Changes to Traffic Control Plans

Requests for modifications to TCPs will be sent to HRM for approval a minimum of 10 days before their proposed implementation.

Section 4.1.7 – Emergency Vehicle Access

Emergency vehicle access to the project site will be maintained at all times throughout the life of the project. There are no fire hydrants within the building footprint or the proposed encroachment areas. The Siamese connection location for the new building is shown on the mechanical drawings submitted for permit and is within the allowable distance to the nearest fire hydrant.

In cases of emergencies, on-site workers will exit the project site through gates along South Street. These gates will remain unlocked at all times when workers are on-site to allow emergency response units access.

Section 4.1.8 – Traffic Control Plans & Haul Route Plan

Traffic Control Plans, Pedestrian management plan, Encroachment Plans, a Haul Route Plan, Barrier Installation & Removal Plans, Services Installation Plans, and Street Closure Plans are provided in the Appendices.

Section 4.1.8.1 – Demolition Phase Traffic Control

There will be demolition completed on this project. Currently, the site has a couple of houses which will be demolished as part of excavation activities.

Section 4.1.8.2 – Construction Phases Traffic Control

During the Demolition, Excavation, Substructure, and Superstructure phases of construction of Wellington Street will not be fenced off or closed to traffic.

The west side of the property area will facilitate loading and deliveries will be accessible from South Street. This area will be the only site access point throughout the construction. Entrance and exit gates will be identified by signage on the gates. There will not be any impact on Wellington Street caused by construction during any phases.

See Appendices A, B and C for details.

These encroachments will require some free on-street parking to become no-parking areas. See Section 4.1.10 for details on parking stall removal.

Section 4.1.9 – Haul Route Plan

A Haul Route Plan (HRP) is provided in Appendix F and will be implemented throughout all phases of this construction project. This HRP includes the proposed route which construction and delivery trucks will use throughout construction and adheres to the HRM Traffic Control Manual Supplement. As shown in the HRP, haul routes will extend to their origins and destinations by streets approved in the HRM Truck Route Bylaw (T-400). The haul route consists of Wellington St, South St, and Inglis Street.

Section 4.1.10 – Parking

Section 4.1.10.1 – Parking Stall Removal

Street parking will be impacted during the service installation and excavation phase where a sidewalk encroachment will be required. During the other phases on the project, there will not be any impact of Parking stalls on Wellington Street.

Section 4.1.10.2 – Contractor Parking

Construction workers will be encouraged to carpool into the construction site from a remote location.

During excavation and substructure phases of the project, the site shall be able to accommodate a few additional vehicles used for carpooling. The suggested carpool location is outside the construction site as shown in Appendix P.

Construction workers shall not be permitted to park within encroachment areas throughout the construction.

Section 4.1.10.3 – Out-of-Service Parking Meter Fees

This project will not impact any parking meters.

Section 4.1.10.4 – Temporary Parking

There is currently no request for HRM to relocate temporary parking. The needs of the community shall be assessed during construction, and should there be a need for additional parking as the project progresses, HRM will be consulted for temporary parking relocation.

Section 4.1.10.5 – Net Parking Loss

During the Excavation, Substructure, and Superstructure Phases, there will not be any loss of spaces both within and opposite the Wellington Street encroachments.

Section 4.1.10.6 – Parking Signage

Signage showing additional parking locations around the neighbourhood is included in the Traffic Control Plans and Pedestrian Management Plan Renderings (See appendix G) These will be mounted prominently around the project site such that they will be identifiable to motorists and pedestrians.

Section 4.1.10.7 – Parking Within Encroachment Areas

Encroachment areas are intended for use as loading areas. On-site workers will not be permitted to park within them.

SECTION 4.2 – PEDESTRIAN MANAGEMENT

A detailed Pedestrian Management Plan (PMP) has been prepared for this CMP. A qualified TWS has developed the plan to meet all applicable regulatory requirements and ensure the safe passage of all pedestrian traffic that enters or exits the streets as well as all pedestrian traffic that travels the streets impacted by the construction project. The PMP is a separate document from the TCP and is concerned with pedestrian routing and pedestrian specific signage.

Eastern Traffic has prepared Pedestrian Management Plans (PMPs) and Pedestrian Management Plan Renderings (PMPPRs), to assist pedestrians in navigating their way around this project site. See Appendices A, B and C.

Section 4.2.1 – Bus Stop Relocation

No bus stops will need to be relocated for this project.

The nearest Bus stops are:

Inglis St Before Tower Rd (6969) (approximately 250m away)

South St Opposite I.W.K Hospital (8303) (approximately 224m away)

Section 4.2.2 – Payment of Applicable Fees

Payment of all applicable fees will be made per HRM Administrative Order 15 (AO15). Requests for lane and street closures must be submitted to HRM for review and approval a minimum of 10 days before the planned implementation.

Section 4.2.3 – Pedestrian Hazard Assessment

See Appendix D for Pedestrian Hazard Assessment information.

Section 4.2.4 – Pedestrian Management Plan Preparation & Monitoring

Separate Pedestrian Management Plans (PMPs) have been prepared for each project phase. PMPs were developed by Eastern Traffic's certified temporary workplace signer (TWS), John Smith. The PMPs illustrate the proposed signage, which will be installed to warn pedestrians ahead of, and throughout the construction zone. Pedestrian management features and methods, as well as information related to the PMPs and Pedestrian Management Plan Renderings, are provided in Section 5.3 and Appendices C and G.

Section 4.2.5 – Notifications of Pedestrian Closures

BIL shall notify HRM and the public of proposed pedestrian closures as outline in Section 8.3.

Section 4.2.6 – Pedestrian Management Plan Compliance

All PMPs will be implemented by Eastern Traffic and will be monitored by BIL. Construction warning signage will be displayed throughout the approaches to and adjacent to the project site. BIL will inspect pedestrian management elements at the start and end of the workday, or more frequently as required and will maintain pedestrian management elements as needed to ensure the PMPs are adequately and correctly implemented. See Appendix E for a sample CMP element inspection sheet to be completed by BIL.

Section 4.2.7 – Changes to Pedestrian Management Plans

Requests for modifications to PMPs will be sent to HRM for approval a minimum of 10 days prior to their proposed implementation.

Notification of pedestrian route disruptions will be distributed to affected residents and businesses a minimum of 5 days in advance of implementation. Modifications to PMPs for unforeseen events will be immediately sent to the Municipality for approval.

Section 4.2.8 – Construction Phases Pedestrian Management

The Excavation Phase will require an approximate 20' deep excavation to construct the parking levels.

As a result of half sidewalk closures, pedestrians will be redirected to the sidewalk on the east side of Wellington. No other businesses or buildings are located along these sections of closed sidewalk, business potentially impacted by the sidewalk closures will be identified in the PMPRs to assist pedestrians in finding these businesses. See Section 5.3. For more information on pedestrian signage.

Throughout the Excavation Phase, a 1.8 m (minimum) high rigid fence (concrete barrier included) will be constructed around the development to prevent pedestrians from entering the site. This fence will be affixed to existing curbs or sidewalks or F-Type concrete barriers.

A Barrier Installation Plan for the Excavation Phase is provided in Appendix A. During barrier installation, the pedestrian routes immediately adjacent to the project site will be closed to the public. Pedestrians will be redirected to the sidewalk on the east side of Wellington.

Any Street closures as outlined in section 4.1 is not expected to affect pedestrian traffic more than as described above.

Section 4.2.9 – Pre-Project Hazard Assessment

See Appendix D for vehicular and pedestrian hazard assessment information.

Section 4.2.10 – Visually Impaired Persons

Construction areas can be particularly challenging to navigate for visually impaired persons. BIL has reviewed elements identified by the Canadian National Institute for the Blind (CNIB) to assist the visually impaired. It has incorporated these elements into the design of the CMP.

Reflective tape with contrasting colors will be provided along the sides of F-Type concrete barriers, near their tops, both on the vehicular traffic side and the pedestrian traffic side.

Sawhorse barricades painted “safety orange” will be located at all termination points along pedestrian routes to assist all pedestrians in identifying the transition between pedestrian routes and traffic travel ways. These sawhorse barricades will be constructed complete with an orange-painted wooden 2” x 4” with signs indicating “No Crossing” which will be fixed along the bottom of the sawhorse barricade for cane detection. Sawhorse barricades will be set up at existing street crossings to identify that these crossings are out-of-service. Sawhorse locations are shown in Appendix C.

Braille text is provided on all Pedestrian Management Plan Renderings (PMPRs) and wayfinding signs. This Braille text will provide emergency contact information. Section 5.3 Appendix G provide additional information regarding these signs.

Section 4.2.11 – Accessibility

All pedestrian routes shall be barrier-free. All site signage not on the encroachment hoarding will be constructed of suitable material, and in such a way to stay out of the HRM ROW and impede vehicle and pedestrian traffic. BIL will inspect the condition of signage daily. The PMP ensures that pedestrian routes are accessible.

SECTION 5.1 – SITE PROTECTION AND HOARDING MATERIALS

Section 5.1.1 – Concrete F-Type Barriers

Concrete F-Type barriers will be installed as per the TCP and PMP drawings. BIL shall regularly inspect the condition and layout of these barriers.

Concrete F-Type barriers shall be assembled so that their “J-J Hooks” interconnect. The sides of the F-Type concrete barriers, along their tops, will have reflective tape with contrasting colors to assist in delineating traffic travel ways and pedestrian routes. This tape will be placed on both the traffic sides and the pedestrian sides of the barriers, as applicable.

Section 5.1.2 – Fencing

Throughout all construction phases, a rigid fence will surround the development. This fence will be drilled and bolted into the surfaces of the Concrete F-Type barriers. The fence structure will be sufficiently strong such that it cannot be moved, removed, or overturned. BIL shall regularly inspect the rigid fences to ensure their continued structural integrity. Any deficiencies in fencing shall be addressed immediately to prevent pedestrians from entering the project site.

Section 5.1.3 – Translucent Mesh

Within 2 m of the public ROW, rigid fencing will be covered with a translucent mesh, a minimum of 1.8 m high, to help control dust and minimize noise.

Section 5.1.4 – Hoarding

Any solid plywood hoarding will be designed and certified by a Professional Engineer (P.Eng.) and mounted on the part of the rigid fence all hoarding will be installed as per HRM specifications and guidelines.

Section 5.1.5 – Covered Ways

There will be no covered ways used on this project.

Section 5.1.6 – Snow Removal

It will be BIL’s responsibility to keep all encroachments clean and free of snow and ice. BIL will not dump snow or ice onto adjacent public property and will truck snow off-site as required to prevent the dangerous build-up of snow piles.

The contractor will be responsible for removing snow and ice as required to ensure that emergency access is maintained to the project site.

Section 5.1.7 – Site Lines

Rigid fences and signage will be installed as per the TCP/ PMP such that vehicular site lines are maintained around the corners of street intersections.

Section 5.1.8 – Emergency Access & Egress

Along South Street, the site will be accessible to emergency vehicles through one gate located at 5846 Lot#13 South St. These gates are the only locations that will receive equipment/materials during construction and will be locked at all times after work hours. On-site workers will exit the project site through these gates. These gates will remain unlocked at all times when workers are on-site to allow emergency response units access to the site.

There are no existing fire hydrants on the project site or within the proposed encroachment areas.

Section 5.1.9 – View Ports

There will be no viewports used on this project as there will be no pedestrian walkways adjacent to the project hoarding/ fencing.

Section 5.1.10 – Reinstatement of Public Property

BIL will be responsible for repairing and paying for any damages incurred due to temporary encroachments including, but not limited to:

- Rigid fence and scaffolding holes reinstated with concrete to existing conditions or better;
- Street lines repainted to existing conditions;
- Damaged sidewalks, curbs, sodding, and other public elements restored to existing conditions or better;
- Cracked asphalt within the encroachment areas repaired using hot rubber (or approved equivalent);

SECTION 5.2 – SITE PROTECTION AESTHETICS

Pedestrian Management Plan Renderings and wayfinding signage (see Section 5.3) will improve project site aesthetics and provide the public with information related to the project and nearby amenities. BIL shall regularly inspect the project site, its signage, and adjacent areas and keep these well maintained.

BIL will also beautify the site hoarding system with rigidly attached banners showing project renderings and sales and marketing information. These banners will be of high-quality and durable material. BIL is looking to apply for a 50% reduction in encroachments fees for site beautification, as suggested in the current CMP administrative order.

SECTION 5.3 – SIGNAGE

Section 5.3.1 – Project Management Plan Renderings

Pedestrian Management Plan Renderings (PMPR) will be mounted at the locations identified on the PMPs to assist pedestrians in navigating around the project site. And to keep the public informed of general project details and project contact information. PMPRs will be located at pedestrian pinch points to give pedestrians advanced warnings to possible obstructions due to construction activities. PMPRs for this project will be at:

- The intersection of Inglis Street & Wellington Street;
- The intersection of South Street & Wellington Street;
- The intersection of Wellington Street & Tower Terrace;
- The intersection of Wellington Street & Lundys Ln;

These PMPRs will be printed on signboards 600 mm x 600 mm in dimension and will be placed at the project site a minimum of 10 days before the commencement of work. PMPRs are provided in Appendix G.

PMPRs reflect the PMPs outlined in Section 4.2. They identify:

- Pedestrian travel routes;
- The position of the project site;
- Street names;
- Names and locations of nearby businesses and points of interest with directional arrows directing pedestrians to them;
- Bus stop locations;
- Emergency contact information, with regular and braille text;
- The pedestrian's current position;

PMPRs will be mounted to existing poles, as shown in Appendix G, pending approval by HRM.

Section 5.3.2 – Pedestrian Detour Wayfinding

During any phases of construction activities, the Wellington Street will be open for all vehicular & Pedestrian movement. But during site services installing time, the side walk will need to be closed down completely. At that time, Wayfinding signage will be incorporated into the PMPR sign at the locations identified on the PMPs. Wayfinding to assist pedestrians in navigating around the project site directing them to local points of interest. These signs will be aesthetically pleasing and are meant to encourage the public to continue using public spaces and visiting local businesses during construction activities. Together with PMPRs, wayfinding signs will be located at pedestrian pinch points to give pedestrians advanced warnings to possible obstructions due to construction activities.

SECTION 6 – LIFTING, HOISTING AND CRANE OPERATIONS

SECTION 6.1 – NAVIGATION CANADA & TRANSPORT CANADA REGULATORY APPROVALS

This project will require a tower crane near the west side of the project site. The top of the crane is expected to be at an elevation of approximately 160' above the sidewalk at the southeast corner of the project site. The crane will swing over the Peter Green Hall Children's Centre as shown in Appendix K.

BIL has reviewed the crane location and height relative to local flight paths and has confirmed the crane will be outside of flight path boundaries. Therefore, Transport Canada approval is not required, and Nav Canada does not require notification.

Information from HRMs technical guidelines on best management practices of construction sites indicate that there are two registered aerodromes in the region; Halifax International and Shearwater airfield. Transport Canada's regulation states that it's applicable to lands that are included in airport zoning regulations. Halifax International Airport zoning regulation describes the outer limit to be a circle with a radius of 4 km centered at the airport, in addition to lands that are under approach surfaces. Shearwater zoning regulations specify areas using landmarks as reference, in addition to lands under approach surfaces. The project site does not fall within these perimeters. Canadian Aviation Regulations- CAR 601.23 deems an object a potential obstacle to air navigation greater than 90m (295') above ground level (AGL). At no time during construction will any equipment or constructed element extend above 90m AGL.

SECTION 6.2 – OPERATIONS ABOVE THE PUBLIC REALM

This project will require a tower crane to construct the concrete structure. The crane will be owned and operated under the direct supervision of a licensed crane operator and formwork contractor. The Crane location is shown on in Appendix K.

A 500-ton mobile crane will be used to erect and disassemble the tower crane in approximately May 2022. The erection of the Crane is intended to take place entirely within the site confines. Traffic Control will be used where required.

When dismantling the crane, the formwork contractor will apply to HRM for a street closure permit. BIL will ensure notification is delivered as per section 8.3 prior to any closure.

All materials required to construct the building will be contained within the site. No loads will cross adjacent property or HRM row at any time during construction.

At the end of the workday, when the crane is out of service, and there is no crane operator on-site, the jib of the crane will be free-swinging, allowing the crane to "weathervane."

Concrete will be poured using the crane with two 4-yard buckets. As the tower crane lifts one full bucket of concrete, the second bucket will be filled from the concrete trucks; This, along with having two trucks inside the encroachment will minimize concrete pouring time.

SECTION 7 – ON-SITE CONDITIONS

SECTION 7.1 – SITE SAFETY AND SECURITY

BIL and associated shareholders have many years of experience constructing high-rise buildings on the peninsula of Halifax and understand the need for proper security controls and a comprehensive safety program.

A third-party safety consultant is used for all BIL projects; They will perform all required site inspections and ensure strict compliance with all applicable safety regulations. BIL will conduct additional daily site safety inspections.

A copy of BIL's safety plan will be submitted to HRM.

All contractors on site will be required to be registered members in good standing with the Nova Scotia Construction Safety Association. Contractors will be required to comply with all applicable safety codes and regulations. BIL will provide a mandatory site safety orientation for all trades and site visitors.

BIL will have certified first-aid responders on-site during all construction activities. First aid kits will be made available at the project site, and site office and locations of first aid kits will be prominently posted and communicated to all on-site workers and visitors. In addition, fire extinguishers and burn kits will remain available on-site at all times. BIL will be responsible for carrying out regular inspections of first aid kits, fire extinguishers, and burn kits and to note any deficiencies and replenish kits as required for any missing or used items.

Section 7.1.1 – Access & Egress Gates

Entry/ exit gates will be locked daily after work hours. These gates will be secured to prevent public access to the site at all times. High pedestrian traffic areas will be monitored during working hours.

Signs identifying the "Entrance Gate" and "Exit Gate" will be prominently posted. These signs will be mounted directly onto the gates using steel cables.

Section 7.1.2 – Hazard Warning Signage

Hazard warning signs will be fastened to the "Entrance Gate" and "Exit Gate" warning personnel of potential hazards and personal protective equipment (PPE) required.

Section 7.1.3 – Gate Locking & Monitoring

Gates will be locked during non-work hours and will be closed at all times when not in use. During holidays and weekends, the contractor will be responsible for checking the project site gates daily to ensure they are secure.

Section 7.1.4 – Hoarding Signage

Hoarding will be marked with "No Trespassing – Construction Personnel Only" signs. All personnel on the construction site will be required to use all proper personal protective equipment (PPE) at all times. PPE requirements will be prominently posted, and visitors will be required to sign in at the project site office before entering the site. A warning of potential fines will be included for those who violate PPE requirements.

Emergency contact information will be prominently posted.

Section 7.1.5 – Inspection Reports

BIL will regularly inspect hoarding and address all safety-related and other deficiencies in a prompt and timely manner. Inspection reports on maintenance activities carried out will be kept on-site at all times.

Section 7.1.6 – Dangerous Activities

Public safety and the safety of on-site workers will be of critical importance throughout all construction phases, and all works will be carried out in accordance with the Nova Scotia Occupational Health and Safety Act. For all dangerous activities, first aid kits will be readily available, as outlined in Section 7.1. See Appendix D for vehicular and pedestrian hazard assessment information.

Section 7.1.6.1 – Hot Works

Hot works will be undertaken a minimum of 3 m inside the project site property boundary. During hot works, BIL will ensure that a first aid kit and fire extinguisher are readily available (in addition to the first aid kits and fire extinguishers identified in Section 7.1) near the work such. Also, hot works will be undertaken away from heavy equipment and heavy equipment routes.

Section 7.1.6.2 – Ignition Source Controls

It will be BIL's responsibility to review potential ignition sources regularly to mitigate the potential for them to ignite. Potential ignition sources include faulty wiring, hot surfaces and motors, welding, grinding, and other sparks, convex lenses (magnifying glasses), and reactive chemicals. Material and equipment specifications and best practices will be followed during all construction activities to reduce the risk of ignition. In addition, potential ignition sources and work, which may result in ignition will be kept away from heavy equipment and heavy equipment routes.

Section 7.1.6.3 – Storage of Combustible Materials

Combustible materials, explosive, reactive, and corrosive materials, will be stored per their product specifications using storage sheds and containers on-site as required and will be kept away from heavy equipment and heavy equipment routes. These materials will be protected from environmental conditions such as snow, rain, and wind to prevent materials from causing harm to on-site workers or the general public.

Section 7.1.6.4 – Waste Management Practices

Throughout construction, BIL will keep a clean and tidy work environment and work to eliminate risks proactively. BIL will monitor the project site throughout the workday to ensure that waste is removed in a prompt and timely manner, ensuring it does not pose a risk to on-site construction activities, on-site workers, or the general public. All waste will be sorted into their appropriate categories, in the interest of cost savings as well to lessen the environmental impact.

Section 7.1.7 – Emergency Contact Information

As outlined in Section 5.3, throughout all project phases, developer and contractor emergency contact information will be prominently posted on Pedestrian Management Plan Renderings and wayfinding signs.

Section 7.1.8 – After-hours Lighting

No after-hours lighting is expected on this project.

Section 7.1.9 – Fire Suppression System

Once the new Siamese connection for the new building has been installed and is operational, access will be provided to the Siamese connection, and signage identifying the Siamese connection "Fire Department Connection – Do Not Block" will be put in place. All Siamese connection will have white backgrounds, bold red lettering, and dimensions of 356 mm x 254 mm. They will be constructed of weatherproof engineering grade reflective aluminum and mounted 600 mm above the top of the Siamese connection.

SECTION 7.2 – MATERIAL HANDLING, LOADING/UNLOADING, DELIVERY, AND VEHICLE STAGING

BIL will adhere to the procedures stipulated in the haul route plan and traffic control plans

Appendices B and F and D for delivery of materials. Delivery vehicles will use designated entry and exit gates.

Timing of deliveries will be coordinated in such a way, that it would have the least impact to regular traffic.

The South Street will be used for delivery trucks, offloading, and concrete operations.

On-site materials will be protected as required from environmental conditions such as snow, rain, and wind to prevent materials from causing harm to on-site workers or the public. Efforts will be made to avoid dust and other materials from becoming airborne during high wind events. See section 7.3.4.

See Appendix E for vehicular and pedestrian hazard assessment information.

SECTION 7.3 ENVIRONMENTAL CONTROLS

Section 7.3.1 – Street & Right-of-Way Cleaning

During the excavation phase and concrete pouring activities, the street will be cleaned daily of any debris from trucks and silt, dirt, or rock that makes its way past the encroachments. A Sweeper Truck will be available and used if required.

Concrete trucks will wash out in a designated washout bin or washout pit to avoid getting cement on their tires and spreading it onto the streets.

BIL will be responsible for removing snow on the street side of F-Type jersey barriers and fences within the ROW that cannot be removed by typical Municipality plowing operations.

Section 7.3.2 – Stormwater Management and Runoff Pollution

BIL will prevent sediment from entering all catch basins adjacent to the project site through the use of erosion and sediment controls (see NSE Erosion & Sedimentation Control Handbook for Construction Sites). All water on-site will be treated and pumped to an adjacent catch basin per HRM Bylaw W-101. The proposed construction will not cause adverse impacts to the storm systems or affect drainage paths.

To achieve this, BIL will install and maintain sediment traps in all catch basins directly adjacent to the project site. BIL has consulted Halifax Water and, with their recommendation, will create a temporary sedimentation pond during the excavation phase of this project. For this phase, BIL will direct stormwater within the project site to a low point where suspended particles in the sediment-laden water will settle out. This water will be pumped out of the project site directly into a catch basin near the west corner of the site. The sediment trap in this catch basin will provide further protection against storm system sedimentation. If high sedimentation levels are detected as a result of Halifax water testing, deflocculants will be on hand.

BIL will apply for a construction dewatering permit with Halifax Water prior to any dewatering activities. This permit will outline any water sampling requirements to be determined by Halifax water. No dewatering activities will take place without approval from Halifax water.

The Stormwater Management Plans for this project (See Civil drawings submitted for permit) identify surface drainage patterns, catch basin locations and runoff quantity and control measures.

Section 7.3.3 – Noise Pollution

BIL will at all times adhere to the HRM Noise Bylaw (N-200). Refer to Section 2 of this report.

Section 7.3.4 – Dust Pollution

Dust mitigation is essential in reducing the impact on the surrounding neighbourhood during construction. BIL will use several strategies to reduce the amount of dust that leaves the site.

During excavation, ‘rock pads’ will be used at the site exit. The rough surface of the rock pads will help to remove most of the mud and dirt from tires and treads of vehicles as they leave the site. The excavation access will regularly be topped with clean gravel to prevent tire tracing from trucks. If debris makes it off-site and onto city streets, they will be swept. In the dry months of the year, and as necessary throughout the project, calcium chloride will be spread on dusty (non-concrete) surfaces, primarily the site entry/ exits where vehicles are likely to ‘kick-up’ dust. This chemical absorbs water from the air and dissolves into the surrounding dirt, thus keeping it moist and preventing dust from getting airborne. If the weather is particularly hot and dry, water trucks will be on standby to spray down any dusty areas if required.

Floors and other surfaces will be cleaned regularly to prevent dust and garbage from blowing it off-site. All sweeping will be done with the use of Dust bane sweeping compound to prevent any dust from becoming airborne.

Section 7.3.5 – Emissions Control

Vehicles will be staged, so idling will not occur for more than 3 minutes at a time. Unless a vehicle motor is required to complete work functions, it will be turned off after no more than 3 minutes. Signs identifying idling requirements will be posted on the front of the project site office trailer and within the loading area.

The arrival of sequential concrete trucks will be timed precisely, so only one vehicle is queuing at a time. To do otherwise would also be costly and unproductive to the owner, contractor, and suppliers.

Section 7.3.6 – Rodent Control

Rodent movement increases during construction activities. BIL has engaged a certified rodent control professional, Target Pest Control, to prepare a Rodent Control Plan (RCP). The RCP applies to all project phases with the goal of preventing movement of rodents off-site to find safe refuge in adjacent areas.

Section 7.3.6.1 – Rodent Control Management

The RCP will consist of a baiting and monitoring program. Bait stations (traps) will be placed approximately every 15 meters (50 feet) as outlined in the NPMA Pest Management Standards for Food Processing & Handling Facilities. Auxiliary buildings will remain locked and secure during all non-work hours and sewer laterals to be removed will be removed in a prompt and timely manner such that they do not provide safe refuge for rodents. In addition, standing water will be promptly pumped off-site to abate attractive conditions for rodent habitation.

Rodent Control R will begin 6 weeks prior to the commencement of the demolition phase by placing 20 bait stations of two levels of underground parking inside the existing buildings on site (to be demolished) and around the perimeter of the project site. This will help to lower the number of active rodents inside the project area prior to demolition. During construction phases (Excavation, substructure, and superstructure), additional bait stations will be set within site and within the HRM ROW encroachments.

Bait stations will be secured in their locations using wooden stakes (for open sodded and dirt locations), weighted patio stones (behind walls and on paved areas), or zip-ties (fixed to fences). Photos of bait station locations are provided in Appendix Q.

Bait stations will be baited using Contrac Blox in accordance with the Environment Canada Pest Control Products Act. Information and specifications for bait stations and bait are provided in Appendix Q.

Target Pest Control will provide monthly monitoring of every bait station and catch basin. During each visit to an individual bait station or catch basin, the Rodent Control R technician will open the bait station (or pull up

the catch basin wire), record the amount of bait consumed, the amount of bait replenished, the site conditions (weather), and the condition of the bait and bait station. BIL will review bait consumption trends to confirm the effectiveness of the RCP. Bait stations may be added in areas of particularly high consumption trends. Visible rodent carcasses will be collected and removed from the project site and neighbouring areas on a regular basis.

A rodent control package will be kept on site at all times and be housed in the project site office trailer. Target Pest Control will send reports to BIL after every visit.

Section 7.3.6.2 – Rodent Control Credentials

Rodent Control Contractors will be certified by the Canadian Pest Management Association (CPMA), be a member in good standing with the National Pest Management Association (NPMA), and certified to be in conformance with ISO 9001:2008. All rodent control technicians on site will hold Nova Scotia Environment (NSE) pest control licenses. Proof of these documents will be kept on-site at the project site office trailer and remain available at all times.

Section 7.3.6.3 – Rodent Control Safety Considerations

Public safety will be of critical importance for rodent control activities. Physical (snap) traps will be prohibited unless approved by HRM (the contractor must request proposed CMP changes to HRM a minimum of 10 days prior to planned implementation for review). Trap size will be sufficiently small to prevent children, dogs, or cats from entering and becoming poisoned and the bait/poison will be sufficiently weak so as not to not kill children, dogs, or cats. In addition, bait stations will be tamper proof and bait will be securely fixed inside so that it cannot be shaken out. Bait stations will be opaque to protect the public from unsightly images.

1.1.1 Section 7.3.7 – Light Pollution

Lighting of the project site (private property) will be 3 m (maximum) above the highest elevation of the building and will be directed inwards towards the property.

SECTION 8 – COMMUNITY ENGAGEMENT & NOTIFICATION

SECTION 8.1 – PRE-CMP COMMUNITY ENGAGEMENT

When developing a new building, there is a potential for disruption to the surrounding community. As such, BIL has been in discussions with surrounding businesses to determine their operational requirements.

To assess the needs of the surrounding community, a consultation letter will be hand-delivered to all civic addresses within a 200m radius of the project site. See Appendix H for a copy of the letter.

A minimum of 10 days before any public ROW disruptions, Project information boards will be posted around the site. Concurrently with work to take place in the public ROW, PMPRs, and wayfinding signage will be posted around the project site as identified in the Pedestrian Management Plans (PMPs) and Appendix G.

SECTION 8.2 – SCHEDULED COMMUNITY NOTIFICATIONS

As mentioned in Section 8.1, monthly project updates will be distributed to those signed up to receive project notifications. These notifications will be sent on or before the fifth day of each month. These notification letters will include:

- The date the letter is sent;
- The development name and owner and contractor contact information;
- Brief updates on project progress;
- Brief updates on expected upcoming construction activities that may affect the community;

- Specific details of any work within the ROW that is to occur outside of the approved encroachment that may result in additional traffic control measures or closures; and
- Any other relevant information.

It will be BIL's responsibility to provide further communication as required by the community (letters, other community consultation meetings, etc.) Copies of all public correspondence will be sent to HRM.

SECTION 8.3 – CLOSURE NOTIFICATION REQUIREMENTS

Notification of street closures and public service interruptions will adhere to the requirements of the HRM Traffic Control Manual Supplement. Street closure requests require 10 days (minimum) notice to HRM prior to implementation and must be approved by HRM before work begins. Notification to the affected public will be made a minimum of 5 days before the disruption. These notifications will be hand-delivered, and BIL will keep and maintain a list of all notified property owners to ensure all affected parties have been notified. BIL will send confirmation to HRM immediately upon delivery of closure notices. A list of respective civic addresses will be included.

SECTION 9 – PERMIT & NOTIFICATION REQUIREMENTS

BIL will be responsible for coordinating a pre-construction meeting 10 days before construction begins. Attendees will include the owner, HRM, Halifax Water (HW), utility companies, and representatives from neighbouring properties. The purpose of this meeting will be to review the CMP on-site with all relevant parties.

SECTION 10 – REGULATION & ENFORCEMENT

SECTION 10.1 – INSPECTION & MONITORING

BIL will be responsible for monitoring the implementation of the CMP on a daily basis, or more frequently as necessary, to ensure its continued effectiveness. The contractor will complete a daily inspection/maintenance log of all CMP elements.

As outlined in Section 1, any changes required to this CMP must be sent to HRM for review 10 days (minimum) prior to their proposed implementation. Changes may only be implemented following HRM approval.

SECTION 11 – SUMMARY

This CMP has been prepared with the goal of minimizing negative impacts on the community, pedestrians, and traffic throughout this project. This CMP will be used as a minimum standard, and any further safety protection required, or methods to provide a more positive environment will be used throughout construction as necessary as approved by HRM.

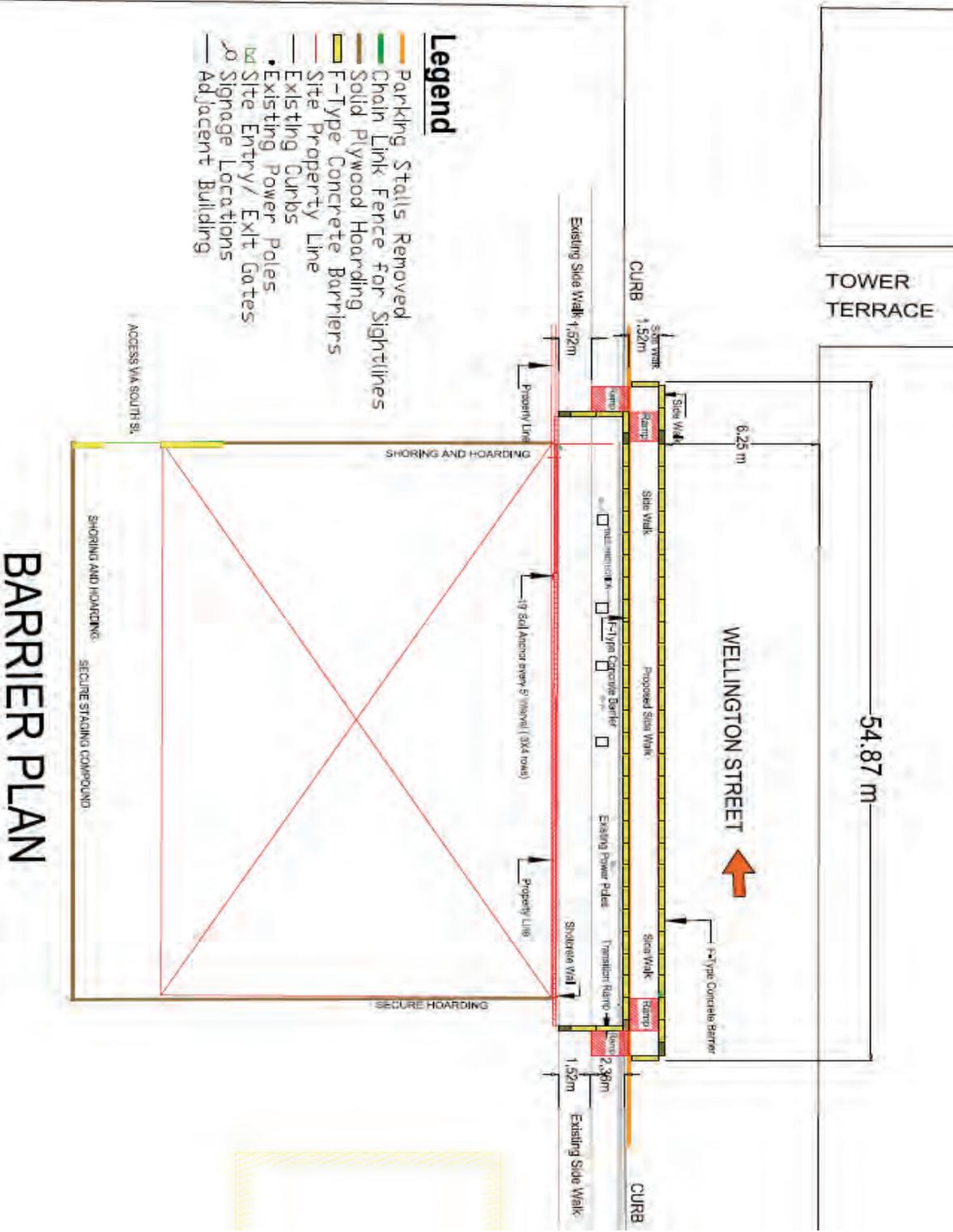
Should you have any questions or comments related to this document, please contact BIL. (see contact information in Section 1.2).

Regards,

Original Signed
Original Signed

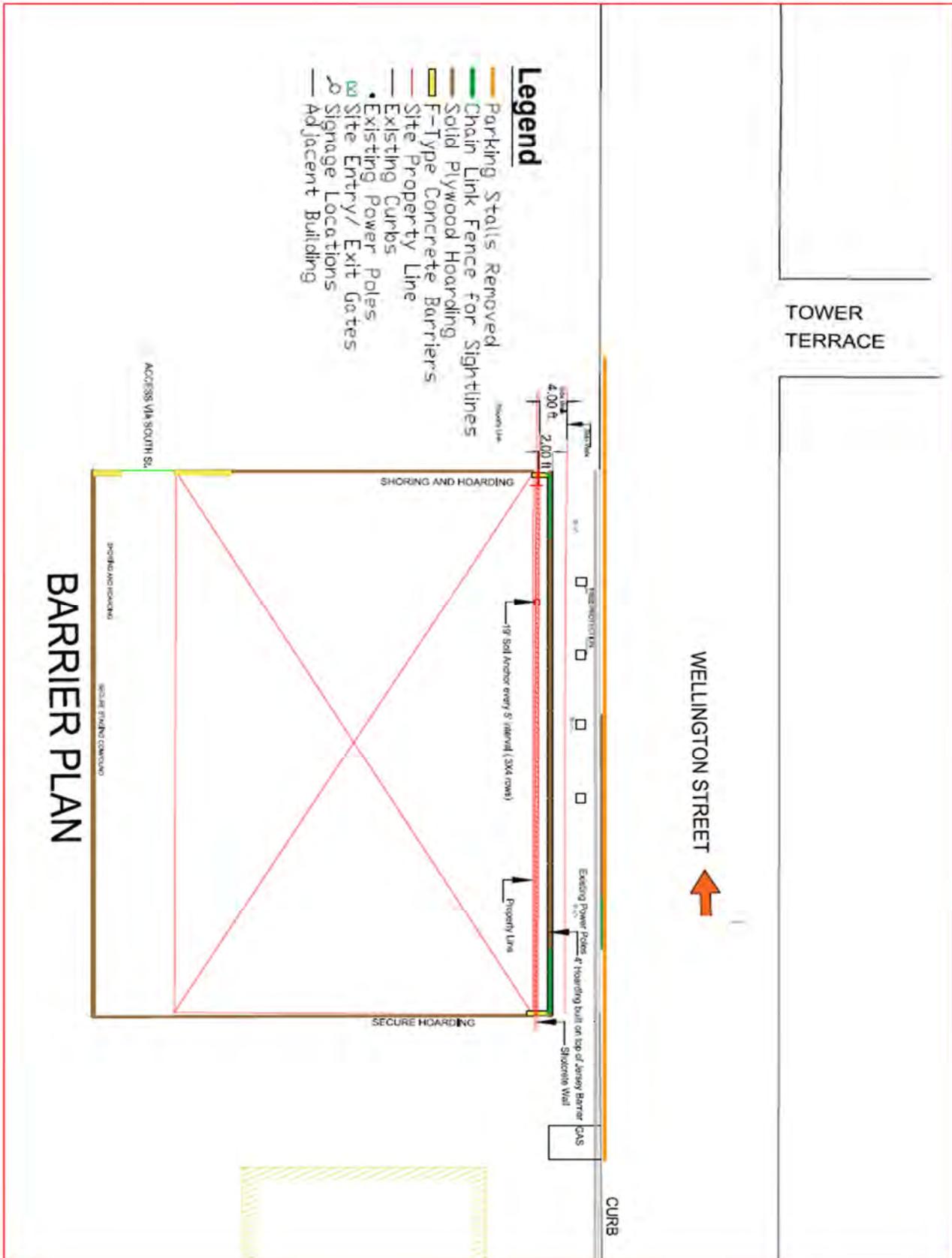
Neil Fougere, P.Eng.
Senior Project Engineer

APPENDIX A – BARRIER PLAN

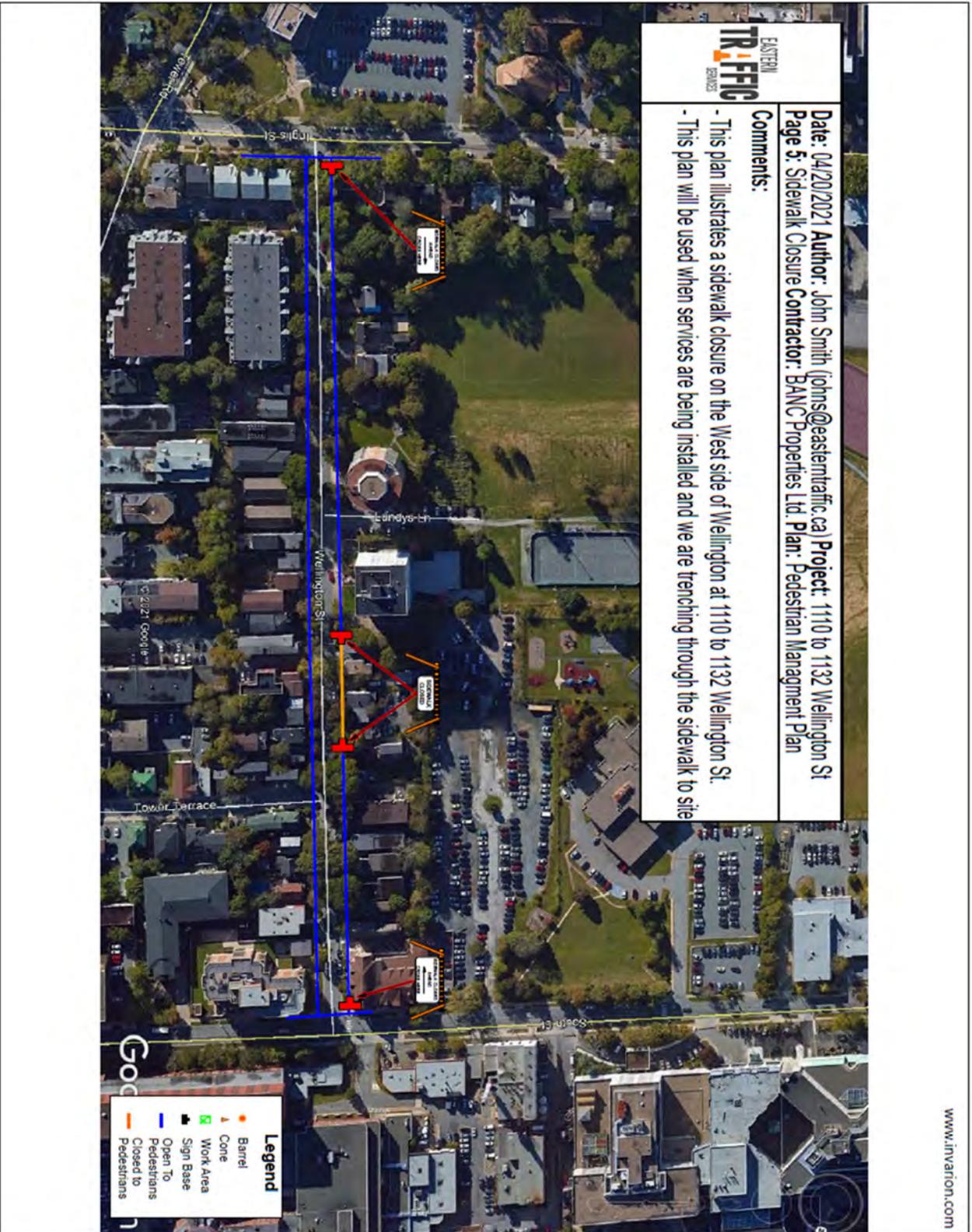


BARRIER PLAN

APPENDIX A – BARRIER PLAN



APPENDIX C – PEDESTRIAN MANAGEMENT PLAN



	Date: 04/20/2021 Author: John Smith (johns@eastentraffic.ca) Project: 1110 to 1132 Wellington St
	Page 4: Temporary Sidewalk Contractor: BANC Properties Ltd. Plan: Pedestrian Management Plan
Comments:	<ul style="list-style-type: none">- This plan illustrates a temporary sidewalk on the West side of Wellington at 1110 to 1132 Wellington St.- Jersey Barriers to delineate the temporary sidewalk using the existing on street parking.



Image © 2021 Navar Technologies

Google Earth

APPENDIX D – HAZARD ASSESSMENT SHEET

Project: Wellington Apartment

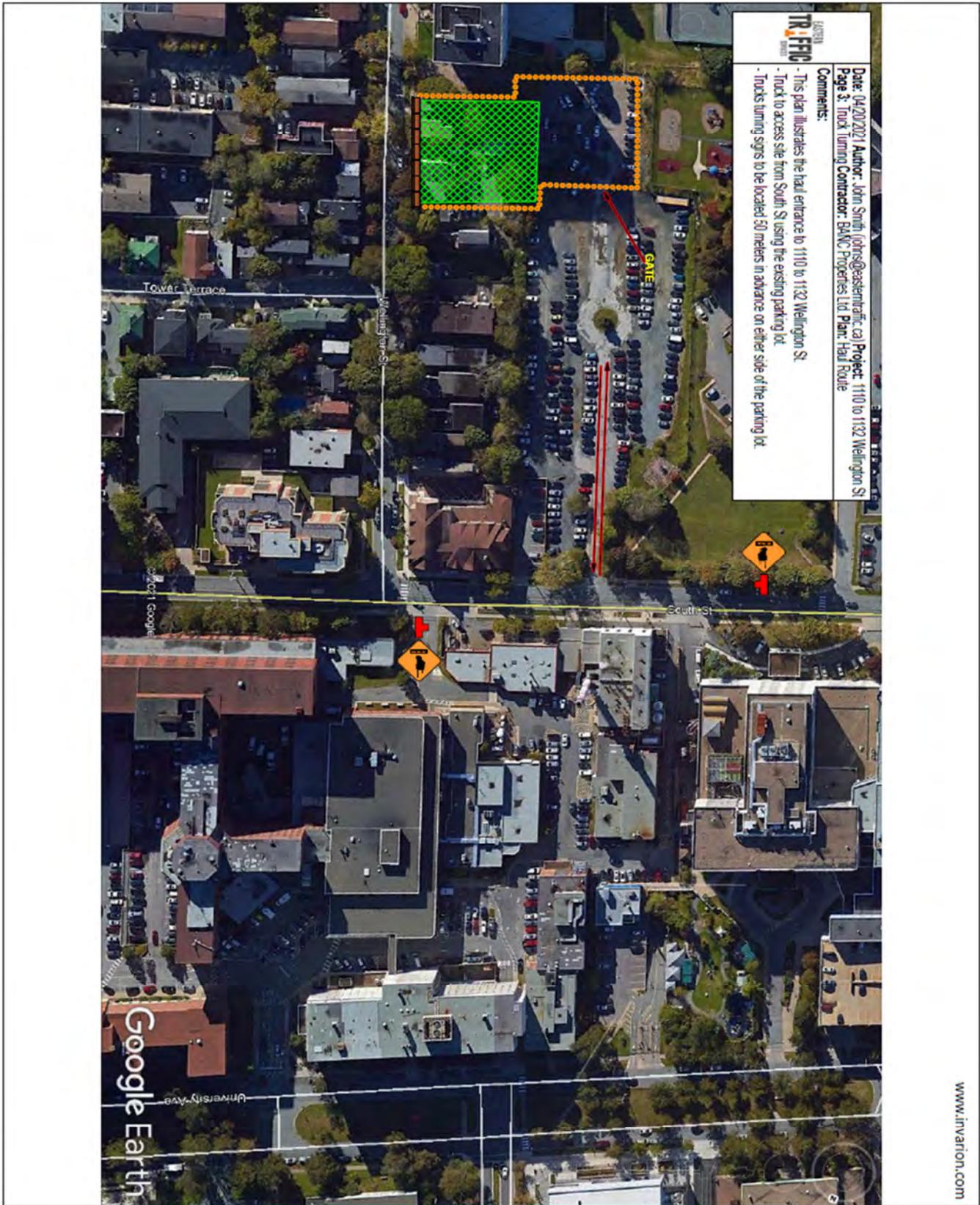
Date: 15/09/2020

Location: 1110 Wellington, Halifax, NS

VEHICULAR & PEDESTRIAN HAZARD ASSESSMENT

No	Hazard:	Project Phase:	Vehicle Impacts:	Mitigation Methods:	Pedestrian Impacts:	Mitigation Methods:
1	Building demolition	Demolition	Debris may fall off building, damaging vehicles.	Clear sidewalks adjacent to site, moving vehicles further away from potential debris.	Debris may fall off building, injuring pedestrians.	Clear sidewalks adjacent to site, moving pedestrians to opposite side of street.
2	10 m deep excavation	Excavation	Vehicles may enter project site and fall down excavation. Vehicle weight may surcharge excavation, causing excavation wall failure.	Place F-Type concrete barriers around entire project site. Clear sidewalks adjacent to project site, moving vehicles further away from excavation.	Pedestrians may enter project site and fall down excavation.	Place F-Type concrete barriers around entire project site.
3	Rock Blasting	Excavation	Blasted rock projectiles may strike vehicles.	Clear sidewalks adjacent to site, moving vehicles further away from blasted rock.	Blasted rock projectiles may strike pedestrians.	Install solid plywood boarding along right fence adjacent to blasting zone.
4	Snow & Ice Cleaning	All Phases	Vehicles may become stuck in snow or slip on ice.	The contractor shall remove all snow on temporary sidewalks and within the loading area and will not dump onto public property (RPM Right-of-Way).	Pedestrians may become stuck in snow or slip on ice.	The contractor shall remove all snow on temporary sidewalks and shall salt sidewalks to prevent ice buildup.
5	Construction Waste	All Phases	Vehicles may strike or be struck by construction waste.	The contractor shall keep the project site and surrounding areas clean and free of construction debris.	Pedestrians may strike or be struck by construction waste.	The contractor shall keep the project site and surrounding areas clean and free of construction debris.
6	Vehicle & Pedestrian Activities	All Phases	Drivers and pedestrians may become confused or impeded with construction activities. Pedestrians may walk in unmarked crosswalks or in vehicular travel areas. Drivers may fail to obey traffic signage.	Vehicular and pedestrian signage will be posted prominently around the project site to facilitate pedestrian movement. Notification will be sent prior to all traffic interruptions.	Drivers and pedestrians may become confused or impeded with construction activities. Pedestrians may walk in unmarked crosswalks or in vehicular travel areas. Drivers may fail to obey traffic signage.	Vehicular and pedestrian signage will be posted prominently around the project site to facilitate pedestrian movement. Notification will be sent prior to all traffic interruptions.
7	Heavy Machinery Operation	All Phases	Heavy machinery or vehicles may break down or overturn, damaging other vehicles.	The contractor shall maintain safe distances between vehicles and heavy machinery on-site. F-Type barriers will be installed to separate construction vehicles from public traffic.	Heavy machinery or vehicles may break down or overturn, injuring pedestrians.	The contractor shall maintain safe distances between pedestrians, vehicles, and heavy machinery. Rigid fences will be installed to separate construction vehicles from pedestrians.
8	Project Site Lines	All Phases	Heavy machinery or vehicles may overturn due to uneven terrain, damaging other vehicles.	The contractor shall maintain safe distances between vehicles and heavy machinery on-site and ensure travel routes are kept flat.	Pedestrians may walk on uneven terrain, causing them to twist their ankles or fall.	The contractor shall maintain safe distances between pedestrians, vehicles, and heavy machinery and ensure travel routes are kept flat.
9	Construction Signage	All Phases	Fences and signs may impact vehicular site line visibility. Construction signage may strike vehicular traffic.	Fences will be curved such that vehicles can see around corners at intersections. Signs will be placed such that they do not extend into vehicle and pedestrian routes.	Pedestrians may walk into construction signage, including traffic signage, pedestrian management plan renderings, wayfinding signs, etc. may.	Signage will be angled in line with pedestrian routes and/or be placed at heights such that they do not pose a risk to pedestrians.
10	Dangerous Materials	All Phases	Flammable, explosive, & hot materials may damage vehicles if not properly maintained & stored.	Construction signage will be securely fixed to existing poles, temporary concrete sign bases, or rigid fences.	Construction signage may strike pedestrians.	Construction signage will be securely fixed to existing poles, temporary concrete sign bases, or rigid fences.
11	Robot Control Devices	All Phases	Flammable, explosive, & hot materials may damage vehicles if not properly maintained & stored.	The contractor will use and store dangerous materials properly as per manufacturers' specifications.	Bar will remain securely fixed inside traps and catch basins, out of reach of the public and pets.	The contractor will use and store dangerous materials properly as per manufacturers' specifications.
12	Hoisting Operations	All Phases	Robot control devices will be placed outside vehicular travel ways and be securely fixed or weighted to prevent unintended movement.	Robot control devices will be placed outside pedestrian travel ways and be securely fixed or weighted to prevent unintended movement.	Pedestrians may trip over robot control devices.	Robot control devices will be placed outside pedestrian travel ways and be securely fixed or weighted to prevent unintended movement.
13	Reinforcement of Public Infrastructure & Service Installation	Superstructure	Precast concrete panels and other items hoisted may fall from heights and damage vehicles.	Proper hoisting and firing techniques will be used to ensure that materials do not fall from heights. F-Type concrete barriers will be installed such that loads are never suspended above the public realm.	Precast concrete panels and other items hoisted may fall from heights and injure pedestrians.	Proper hoisting and firing techniques will be used to ensure that materials do not fall from heights. Temporary sidewalks will be installed such that loads are never suspended above the public realm.
14	Fallen debris	Superstructure	Heavy equipment and hot concrete used during public infrastructure reinstatement and service installation may cause damage to vehicles. Debris may fall from upper stories of the new building causing damage to vehicles.	The contractor shall maintain safe distances between vehicles and heavy machinery on-site. F-Type barriers will be installed to separate construction vehicles from public traffic during public infrastructure reinstatement and service installation.	Heavy equipment and hot concrete used during public infrastructure reinstatement may injure pedestrians.	The contractor shall maintain safe distances between pedestrians, vehicles, and heavy machinery. Sidewalks adjacent to the public infrastructure reinstatement and service installation.

APPENDIX F – HAUL ROUTE PLAN

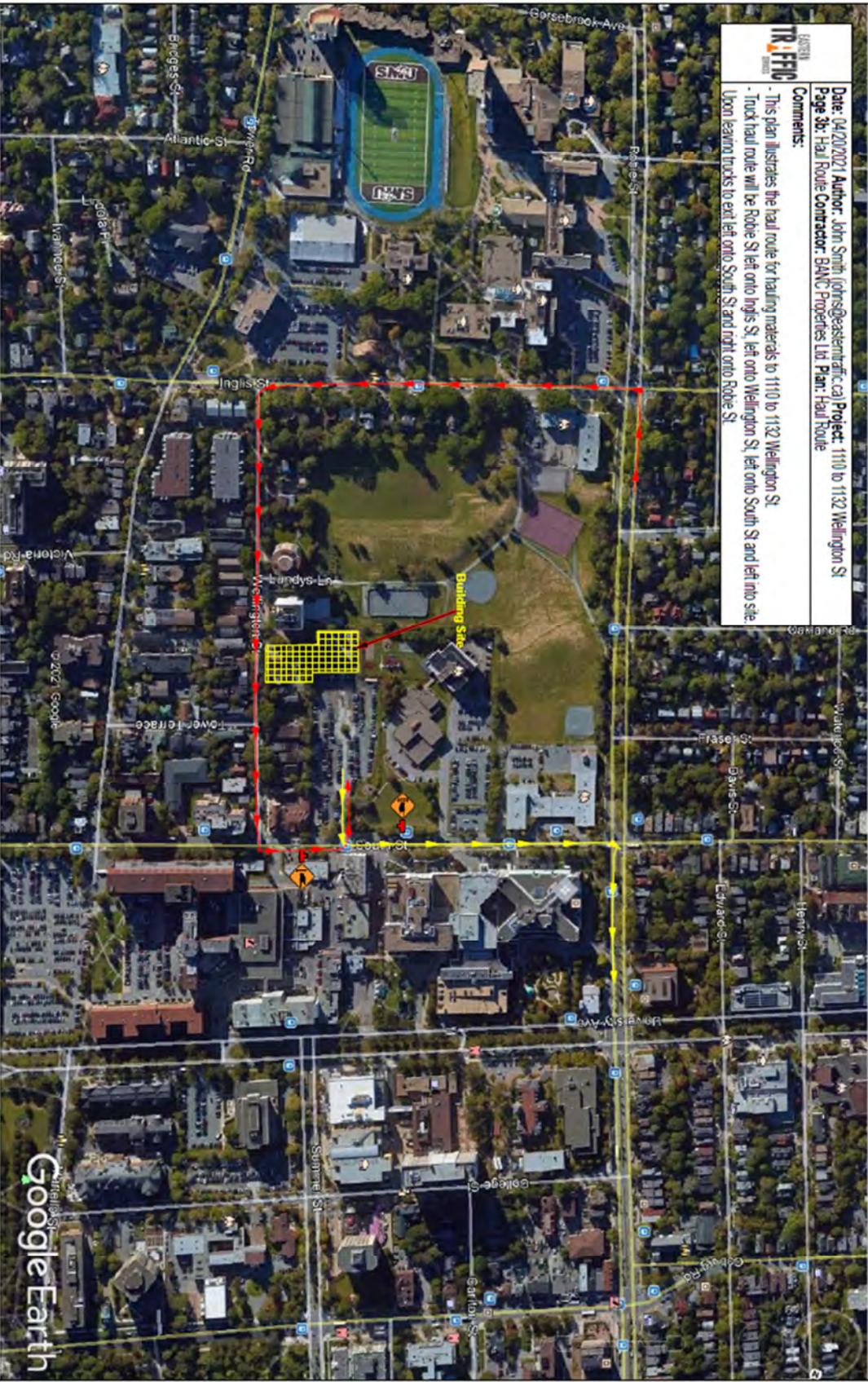


INVARION TRAFFIC

Date: 04/20/2021 **Author:** John Smith (johns@semitraffic.ca) **Project:** 1110 to 1132 Wellington St
Page 3b: Haul Route **Contractor:** BANC Properties Ltd **Plan:** Haul Route

Comments:

- This plan illustrates the haul route for hauling materials to 1110 to 1132 Wellington St
- Truck haul route will be Robe St left onto Inglis St, left onto Wellington St, left onto South St and left into site
- Upon leaving trucks to exit left onto South St and right onto Robe St



APPENDIX G – WAYFINDING SIGNAGE/ PMP



sojournsigns.ca

Dartmouth Tel: 902 468 6892

Client: Banc
Designer: Paul
Date: Sept. 22, 2020
Salesperson: Sean

Approved for Fabrication
 Approved as Noted
 Revise and Resubmit
 Rejected

File Name: _____
 Name: _____
 Signature: _____
 Date: _____

Your signature authorizes Sojourn to complete the sign in accordance with the proof
 This drawing is given in confidence and may not be used or disseminated in any way without prior written consent from Sojourn Signs. All common law and copyright laws are hereby specifically reserved



Sign 1, Wellington & Lundy's Ln.

*This document should not be used for colour matching as each monitor / printer will produce colours that differ.

Details

Sign Type: Wayfinding (Pedestrian)
Quantity: _____

- EG DG HIP Non-Reflective
- Single-sided Double-sided
- Non-illuminated Backlit

Surface: 1st Interior Exterior

Substrate / Structure

- A 0.5" crezon plywood
- B
- C
- D

Graphics

- Vinyl Digital Print Screen Print
- 1 3M L339 with 8508 gloss laminate
- 2
- 3
- 4

Notes: _____

- Installation Interior Exterior

Install Location: _____

Client: Banc
Designer: Paul Salesperson
Date: Sept. 22, 2020

File Name:

- Approved for Fabrication
- Approved as Noted
- Revise and Resubmit
- Rejected

Name _____

Signature _____

Date: _____

Your signature authorizes Sojourn to complete the sign in accordance with the proof
 This drawing is given in confidence and may not be used or disseminated in any way without prior written consent from Sojourn Signs. All common law and copyright laws are hereby specifically reserved

Details

Sign Type: Wayfinding (Pedestrian)

Quantity:

- EG DG H/P Non-Reflective
- Single-sided Double-sided
- Non-Illuminated Backlit

Surface: 1st Interior Exterior

Substrate / Structure

A 0.5" crezon plywood

B

C

D

Graphics

Vinyl Digital Print Screen Print

1 3M L339 with 8508 gloss laminate

2

3

4

Notes: _____

Installation Interior Exterior

Install Location: _____



Sign 2, Wellington & Inglis St.

Client: Banc **Designer:** Paul **File Name:** _____
Date: Sept. 22, 2020 **Salesperson:** Sean

Approved for Fabrication
 Approved as Noted
 Revise and Resubmit
 Rejected

Name: _____
 Signature: _____
 Date: _____

Your signature authorizes Sojourn to complete the sign in accordance with the proof
 This drawing is given in confidence and may not be used or disseminated in any way without prior written consent from Sojourn Signs. All common law and copyright laws are hereby specifically reserved

Details

Sign Type: Wayfinding (Pedestrian)

Quantity:

- EG DG HIP Non-Reflective
- Single-sided Double-sided
- Non-Illuminated Backlit

Surface: 1st Interior Exterior

Substrate / Structure

- A 0.5" crezon plywood
- B _____
- C _____
- D _____

Graphics

- Vinyl Digital Print Screen Print

- 1 3M L39 with 8508 gloss laminate
- 2 _____
- 3 _____
- 4 _____

Notes:

- Installation Interior Exterior

Install Location: _____



Sign 3, Wellington & Tower Terr.

Client: Blanc **Designer:** Paul **File Name:**
Date: Sept. 22, 2020 **Salesperson:** Sean

Approved for Fabrication
 Approved as Noted
 Revise and Resubmit
 Rejected

Name: _____
 Signature: _____
 Date: _____

Your signature authorizes Sojourn to complete the sign in accordance with the proof.
 This drawing is given in confidence and may not be used or disseminated in any way without prior written consent from Sojourn Signs. All common law and copyright laws are hereby specifically reserved.

Details

Sign Type: Wayfinding (Pedestrian)
Quantity:

- EG DG HIP Non-Reflective
- Single-sided Double-sided
- Non-Illuminated Backlit

Surface: 1st Interior Exterior

Substrate / Structure

- A 0.5" crezon plywood
- B
- C
- D

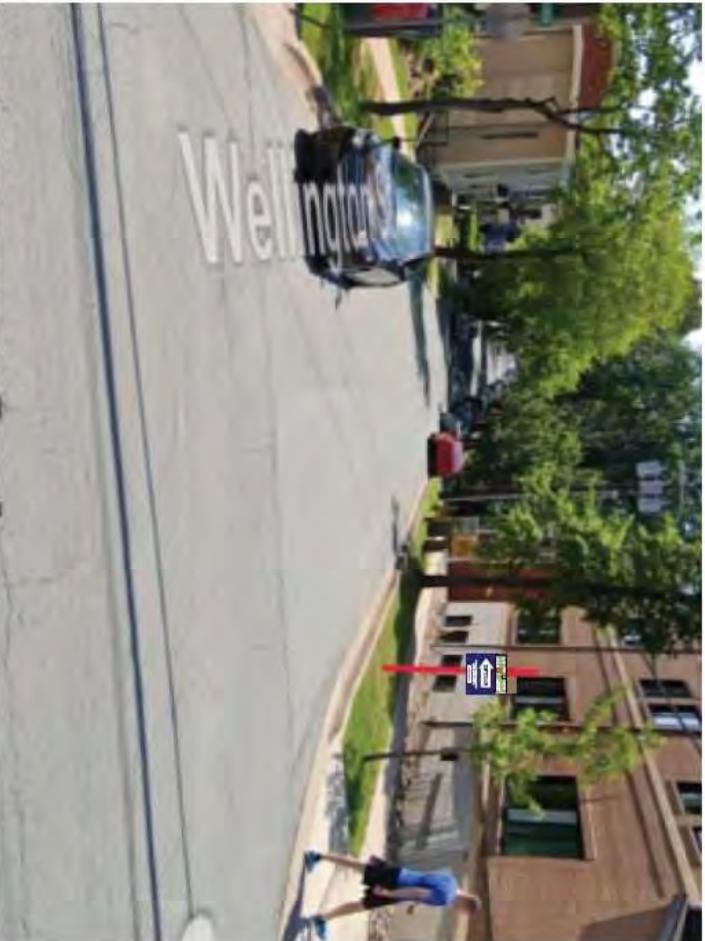
Graphics

- Vinyl Digital Print Screen Print
- 1 3M L39 with 8508 glass laminate
- 2
- 3
- 4

Notes:

Installation Interior Exterior

Install Location:



Sign 4, Wellington & South St.

APPENDIX H – COMMUNITY CONSULTATION LETTER

Sent Via: Hand Delivery

Re: LOCK Suites

Address of Project: 1110-1132 Wellington Street, Halifax NS

1 Craigmore Drive, Suite 201

Halifax, NS, B3N 0C6

902-832-8930 | F: 902-465-8640

www.bancgroup.ca

To Whom it May Concern,

BANC Investment Ltd. is in the final stages of acquiring a building permit for the construction of the project.

We would like to inform you that there may be changes to traffic and pedestrian management and want you to be aware of that as we move forward with construction. BANC Investment Ltd. will do everything within its capacity to mitigate all impacts that are due to arise as a result of said development during construction.

There will be additional correspondences as we move forward with construction and have set up a website for all interested parties to see how we are progressing. Please visit us at **Website:** www.locksuites.ca for project notifications and information.

We will be doing monthly project notifications. Also, should you have any questions, concerns or inquires for future rental, please feel free to contact the below.

BANC Investment Ltd.

Phone: (902) 832-8930

Email: reception@bancgroup.ca

Sincerely,

The Construction Management Team

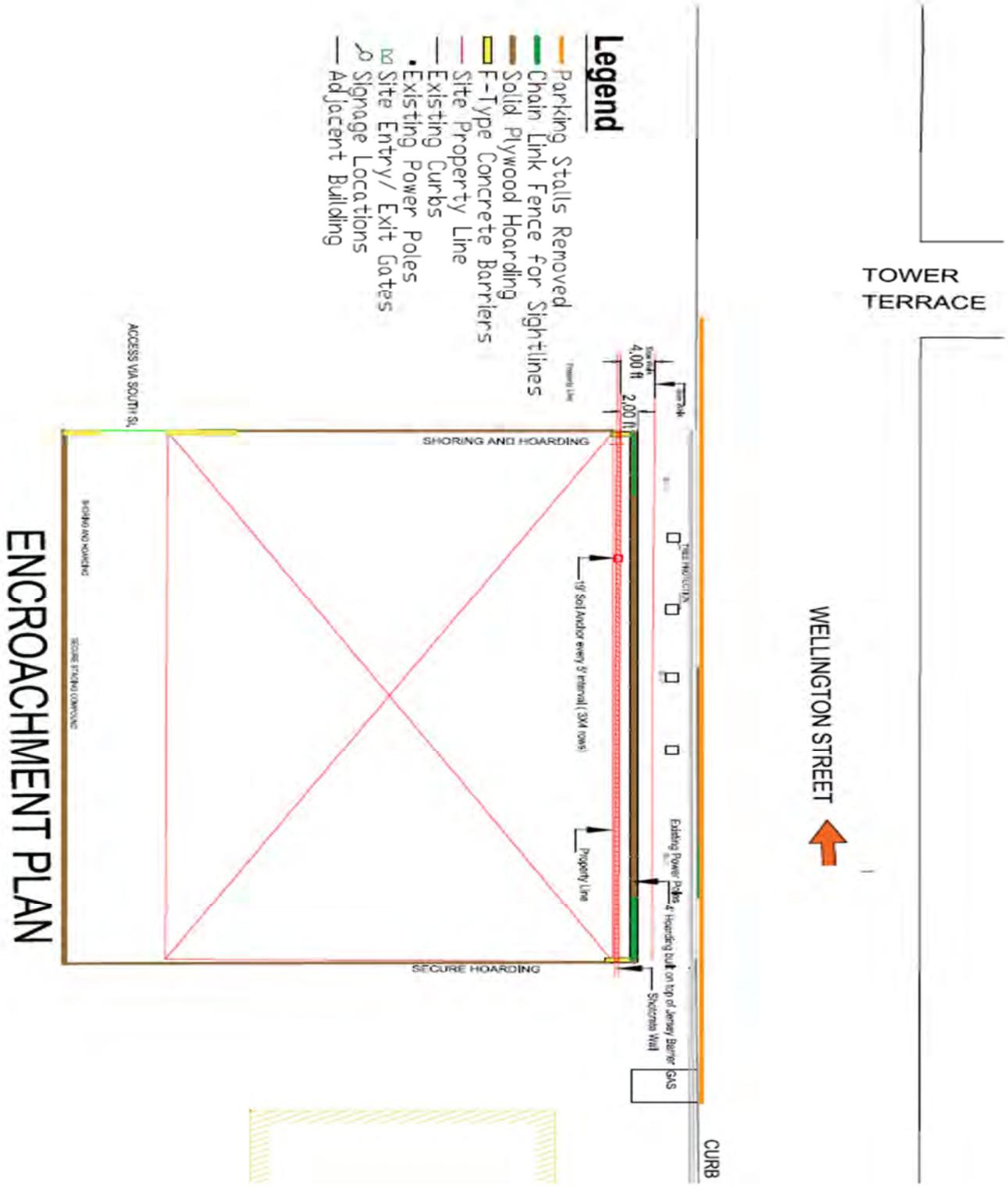
Colin Cameron

Project Manager at LOCK Suites

(902) 221-5526

ccameron@bancgroup.ca

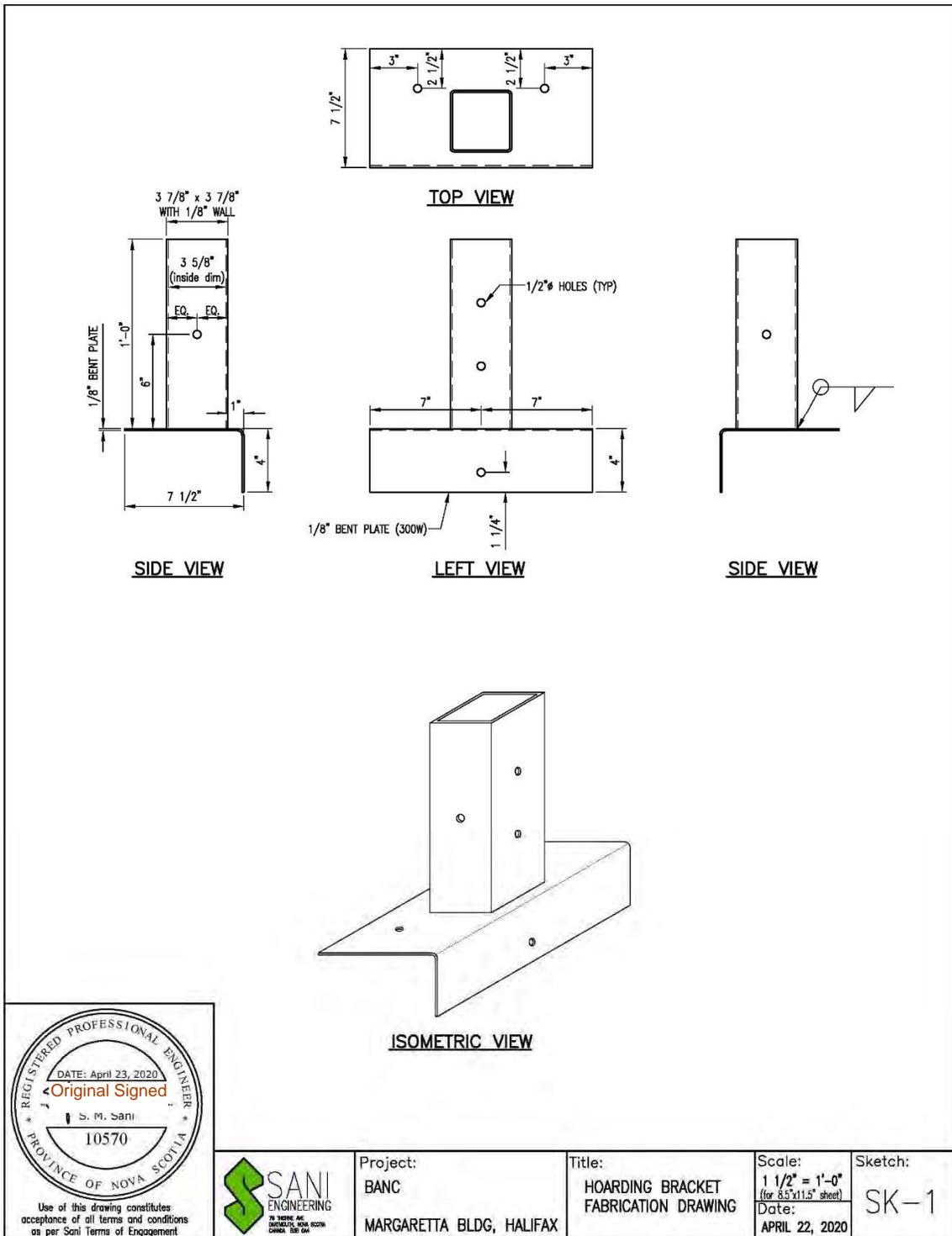
APPENDIX I – ENCROACHMENT PLAN



- Legend**
- Parking Stalls Removed
 - Chain Link Fence for Sightlines
 - Solid Plywood Hoarding
 - F-Type Concrete Barriers
 - Site Property Line
 - Existing Curbs
 - Existing Power Poles
 - Ⓜ Site Entry/ Exit Gates
 - Ⓜ Signage Locations
 - Adjacent Building

ENCROACHMENT PLAN

APPENDIX J – BARRIER BRACKET DETAIL



REGISTERED PROFESSIONAL ENGINEER
 DATE: April 23, 2020
 Original Signed
 S. M. Sani
 10570
 PROVINCE OF NOVA SCOTIA
 Use of this drawing constitutes acceptance of all terms and conditions as per Sani Terms of Engagement



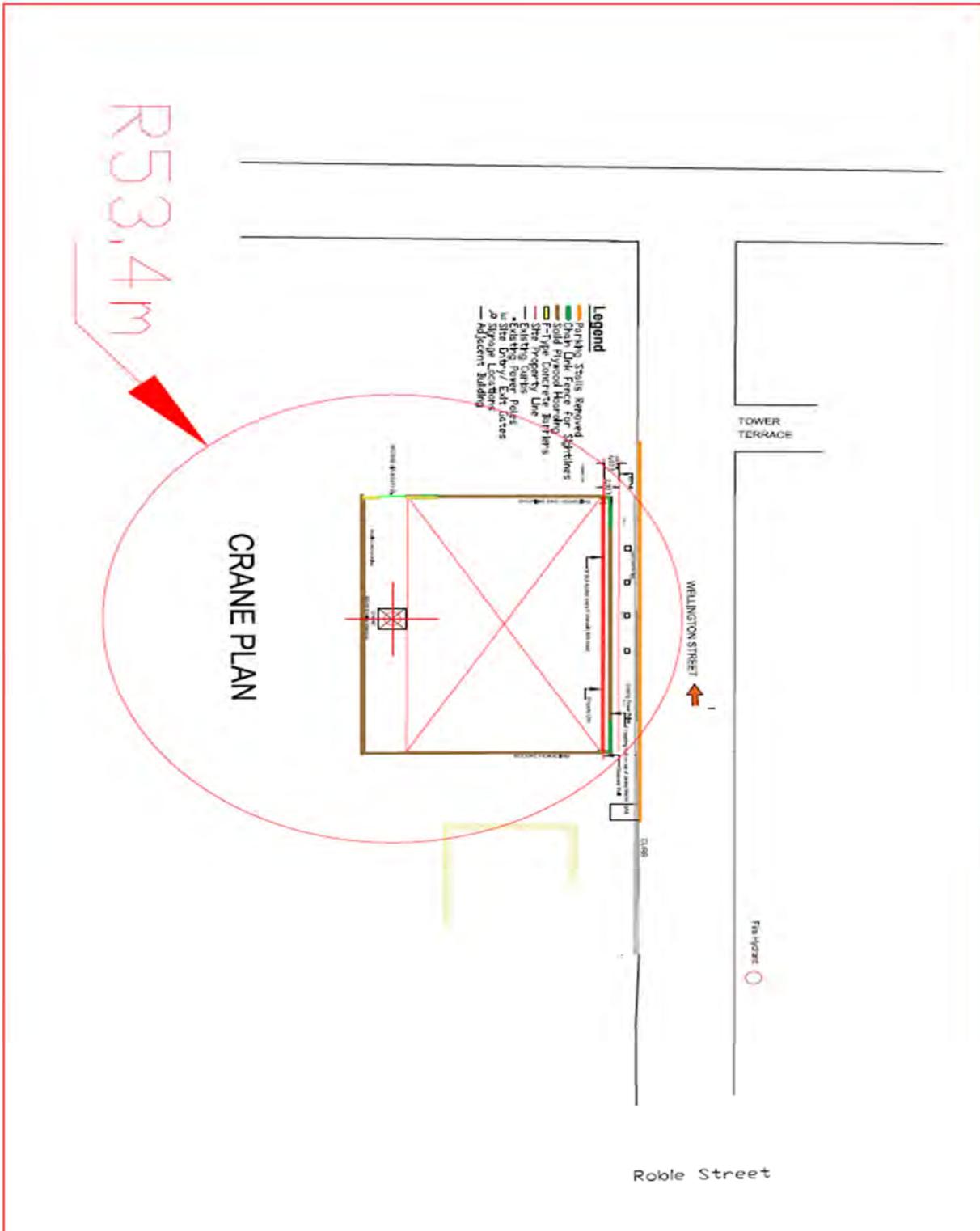
Project:
 BANC
 MARGARETTA BLDG, HALIFAX

Title:
 HOARDING BRACKET
 FABRICATION DRAWING

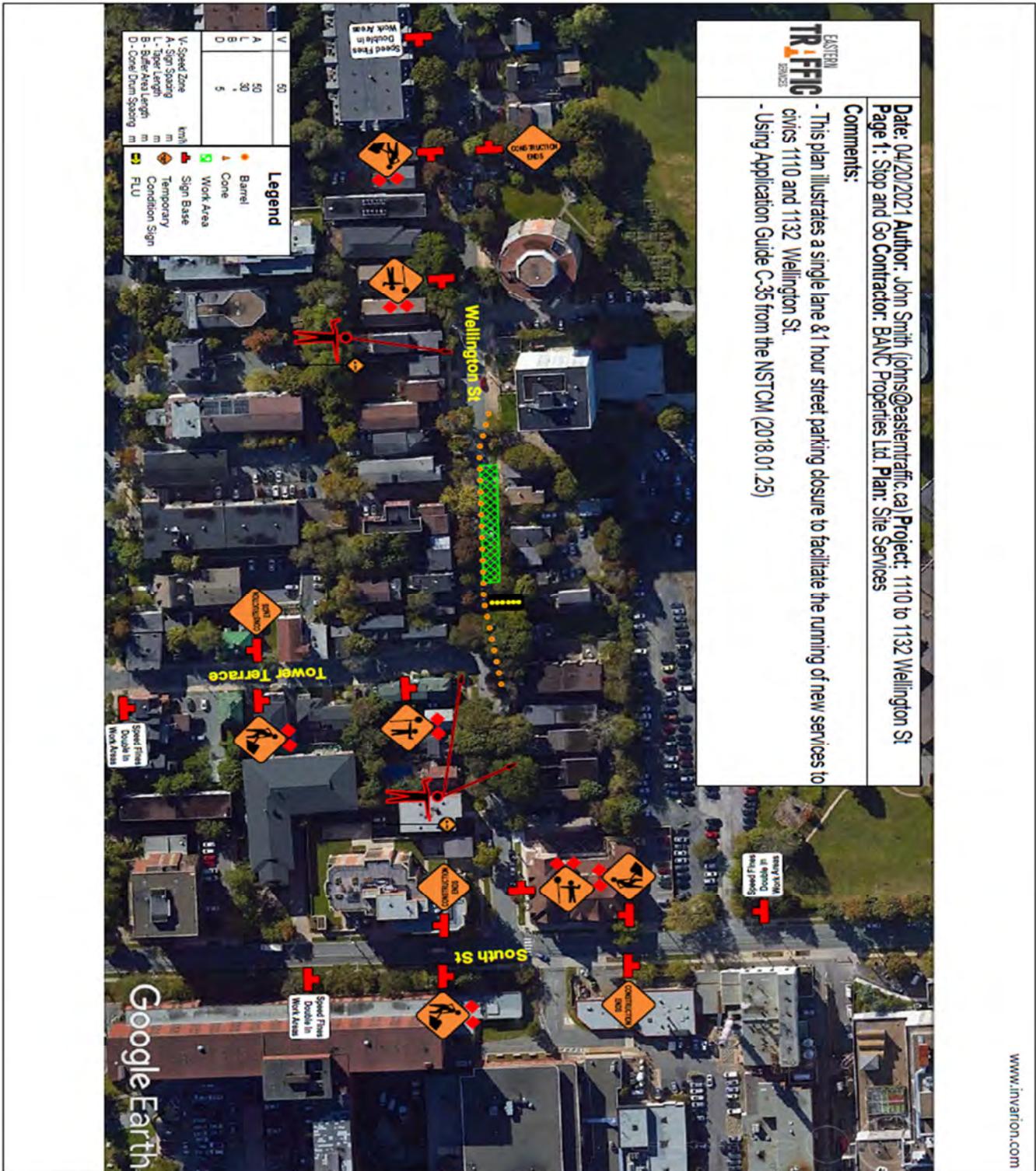
Scale:
 1 1/2" = 1'-0"
 (for 8.5"x11.5" sheet)
 Date:
 APRIL 22, 2020

Sketch:
 SK-1

APPENDIX K – CRANE PLAN



APPENDIX M – SERVICES INSTALLATION CLOSURE



APPENDIX N – OTHER SIGNAGE/DEVELOPMENT INFORMATION SIGN


sojournsigns.ca Dartmouth Tel: 902 468 6892

Client: Banc	Designer: Paul	File Name: BancConstSigns V2	<input type="checkbox"/> Approved for Fabrication <input type="checkbox"/> Approved as Noted <input type="checkbox"/> Revise and Resubmit <input type="checkbox"/> Rejected	Name _____ Signature _____ Date: _____	Your signature authorizes Sojourn to complete the sign in accordance with the proof This drawing is given in confidence and may not be used or disseminated in any way without prior written consent from Sojourn Signs. All common law and copyright laws are hereby specifically reserved
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FIRE
DEPARTMENT
CONNECTION



FIRE
DEPARTMENT
CONNECTION

IDLING
3 MINUTES
MAXIMUM

24" x 24"

1110 Wellington Avenue, Halifax, NS
9-Story residential building with two levels of underground parking.



January 2021 - December 2023

RESIDENTIAL BUILDING

Developer Contact Information

1 Craigmere Drive
Suite 201 Halifax, NS B3N-0C6
Reception
(902)832-8930

24h Emergency Contact Info:

24" x 36"

*This document should not be used for colour matching as each monitor / printer will produce colours that differ.

Details

Sign Type: Coroplast

Quantity: _____

EG DG HIP Non-Reflective
 Single-sided Double-sided
 Non-Illuminated Backlit

Surface: 1st Interior Exterior

Substrate / Structure

A 4mm coroplast

B

C

D

Graphics

Vinyl Digital Print Screen Print

1 3M IJ39 with 8508 gloss laminate

2

3

4

Notes: _____

Installation Interior Exterior

Install Location: _____

APPENDIX O- DEVELOPMENT INFORMATION SIGN

1110 Wellington Avenue, Halifax, NS

9-Story residential building with two levels of underground parking.



January 2021- December 2023

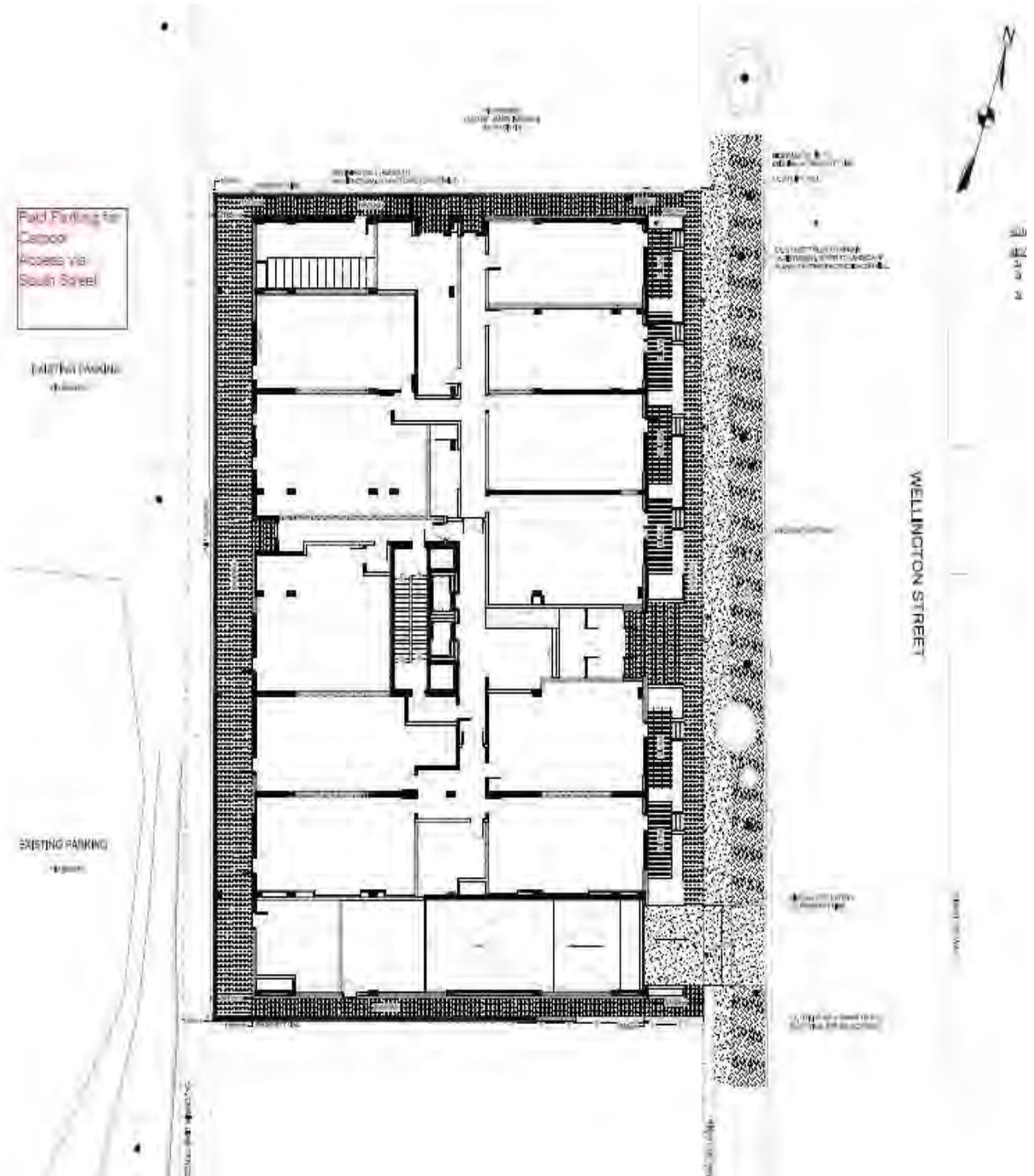
RESIDENTIAL BUILDING

Developer Contact Information

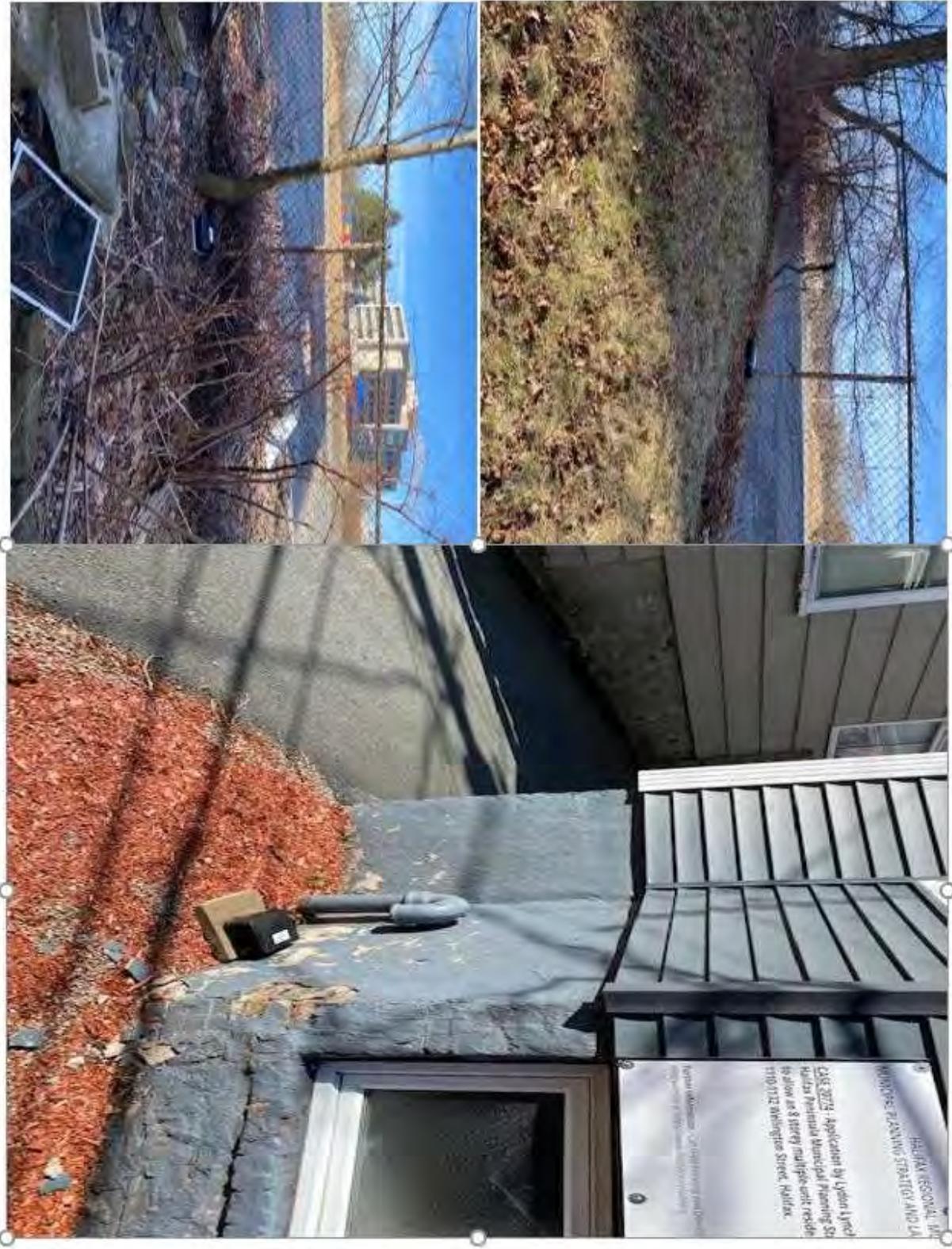
1 Craigmore Drive
Suite 201 Halifax, NS B3N-0C6
Reception
(902)832-8930

24h Emergency Contact Info:

APPENDIX P – CARPOOL PARKING LOCATION



APPENDIX Q – PHOTOS OF BAIT STATION LOCATIONS



APPENDIX R – SIGNAGE INSTALL HARDWARE

Item	Description
Custom Order	TC-64 A Light Barricades
120x60-P21C	120 x 60 cm TC-145 Sidewalk Closed Sign - Crezon Plywood - Engineer Grade Prismatic
120x60-P21C	120 x 60 cm TC-145A Sign - Crezon Plywood - Engineer Grade Prismatic (2) Left (2) Right
120x60-P21C	120 x 60 cm Custom Sign - Crezon Plywood - Engineer Grade Prismatic - SIDEWALK CLOSED AHEAD - USE OTHER SIDE - NO ARROWS - BLACK ON WHITE
TC2-P22C	90x90 cm Road Work Sign - Crezon Plywood - High Intensity - WITH FLAGS
TS1050	12" Flag - 16" x 1" Lexan Flat Staff
TC4-P22C	90x90 cm Construction Ends Sign - Crezon Plywood - High Intensity - Digital Print
TC34-212	90x90 cm Road Narrows Sign - Crezon Plywood - High Intensity
45x60-P22C	60 X 45 cm TC-12S Sign - Crezon Plywood - High Intensity
45x60-P22C	60 X 45 cm TC-11 Sign - Crezon Plywood - High Intensity
45x60-P22C	60 X 45cm TC-11R Sign - Crezon Plywood - High Intensity
45x60-P22C	60 X 45 cm TC-11L Sign - Crezon Plywood - High Intensity
TC171-212	90x60 cm Speed Fines Double in Work Areas Sign - Crezon Plywood - High Intensity
60x60-P22C	60 x 60 cm RB-11L Sign - Crezon Plywood - High Intensity
60x60-P22C	60 x 60 cm RB-11R Sign - Crezon Plywood - High Intensity
TS1934-14	2" x 10" Square Tube Post Kit - 14 Gauge c/w Base and Corner Bolt kit
1037	8" x 8" Steel Base Plate Stand for Square Tube - Powder coated
1933-14G	2" x 8" Square Tube Post Only - 14 Gauge 14.6 lbs.
TS1970	Corner bolt kit - 2 1/2" Corner Bolt c/w flange nut for 2" Square Tube
	24" x 36" 4mm Coroplast Signs - 3MIJ-39 digitally Printed Graphics with 3M 8508 Glass Laminate - Grommets in all 4 corners for zip tying to fences, etc. - Graphics as per proof. Building Rendering - Site Contact info, etc.
	24" x 24" 4mm Coroplast Signs - 3MIJ-39 digitally Printed Graphics with 3M 8508 Glass Laminate - Grommets in all 4 corners for zip tying to fences, etc. - Graphics as per proof. (2) DANGER - DO NOT ENTER (2) PPE SIGNS (2) HAZARDS SIGNS (2) ENTRY (1) EXIT (4) SIAMESE CONNECTION WITH ARROW LEFT (1) SIAMESE CONNECTION NO ARROWS IDLING 3 MINUTES MAXIMUM
HAS-188 D021	36" Adjustable Sign Mounting Strap Flared Bracket - Stainless Steel c/w Hex Bolt and Washer - Straps To 4" + Round, 2-1/2" + Square
Custom Order	24" x 24" Custom 0.080 Aluminum Signs - 3M IJ- 39 Digitally Printed Graphics with 3M 8508 Glass Laminate - GRAPHICS AS PER PROOFS. Proofs to be supplied by Paul - SQUARE.